

ADDENDUM No. 1
Reconstruct GA Apron – Phase 1 Construction
March 12, 2024

This addendum forms a part of the Contract Documents and modifies the original Bidding Documents dated February 28, 2024. Acknowledge receipt of this addendum in the space provided on the bid form, failure to do so may subject the Bidder to disqualification.

General Items

- **Plan Holders List**
The plan holders list can be found on the Rapids Reproductions Plan Room: [Foth # 3-19-0028-081 - Dubuque Regional Airport - Reconstruct GA Apron - Phase 1 :: Rapids Reproductions Online Planroom \(rapidsreproplanroom.com\)](#)
- **Pre-Bid Meeting Minutes**
The Pre-Bid meeting minutes, along with attendees list, is attached to this Addendum. Questions asked during the pre-bid meeting are included within the question and answer log.
- **Contractor Questions**
Questions related to the bidding documents were asked either through e-mail, telephone correspondence, or during the Pre-Bid meeting. Answers to questions that warrant additional clarification have been included within the attached question and answer log.

Specification

Section 1 – Notices, Instructions to Bidders

- **Instructions to Bidders**
 - **REVISE** Article 6.03 to “*All questions must be received no later than 5:00 p.m. local time, March 15, 2024. Questions received after this date may not be answered.*”

Section 2 - Bid Forms

- **Proposal Form**
 - **REVISE** Item No. 4 *Drainage Pipe Removal, Less Than 18 Inch Dia.* quantity to 730 LF.
 - **REVISE** Item No. 11 to “*Remove and Stockpile Contaminated Soil*”, unit of measurement to CY and quantity to 500 CY.
 - **Pages 1 and 2 of the proposal form have been revised and included as an attachment to Addendum 1. Contractors shall replace the first two pages of the original proposal with the two sheets modified by this addendum. No other changes have been made to the proposal form or proposal attachments.**

Section 3 – Contract Forms

- None

Section 4 – General Provisions

- None

Section 5 – Supplementary Provisions

- None

Section 6 – General Construction Items

- None

Section 7 – FAA Technical Specifications

- None

Section 8 – Supplementary Specifications

- None

Appendices

- None

Plans

- **Plan Sheet B.02 – Typical Cross Sections and Details**
 - **REVISE** Detail 03 to “Type “A-2” – 1 Inch Isolation Joint”.
 - **ADD** Joint width dimension of 1/2” +/- 1/16” to Detail 07 *Construction Joint Sealing Detail* and Detail 08 *Contraction Joint Sealing Detail*.
- **Plan Sheet C.01 – Project Quantities and Tabulations**
 - **REVISE** Item No. 4 *Drainage Pipe Removal, Less Than 18 Inch Dia.* estimated Division 1 quantity to 730 LF and estimated Total quantity to 730 LF.
 - **Revise** Item No. 11 to “*Remove and Stockpile Contaminated Soil*”, unit of payment to CY, estimated Division 2 quantity to 500 CY and estimated Total quantity to 500 CY.
- **Plan Sheet C.02 – Estimate Reference Information**
 - **REVISE** Item 10 Reference Note B to “All excess excavation material including unsuitable and non-contaminated material shall become property of the contractor. No disposal site exists on airport property other than for materials identified as contaminated per Bid Item 11.”
 - **REVISE** Item 10 Reference Note C to “Excess excavation material classified by the RPR and environmental testing firm as contaminated shall be handled and stockpiled as described in Bid Item 11. The quantity of contaminated soil stockpiled on airport property shall be subtracted from the estimated unclassified excavation volume.”
 - **ADD** Item 10 Reference Note D “Incidental to this bid item, it is anticipated that hauling to the stockpile area may be necessary to confirm the excavated material’s contamination Status. Should the material be found non-contaminated, additional payment for hauling off-site will not be received.
 - **REVISE** Item 11 Reference Note B to “Contractor shall manage, handle, and stockpile contaminated soils and groundwater per the recommendations of the Soil and Groundwater Management Plan (SGMP). This item is measured per cubic yards (CY) of contaminated soils that require the contractor to stockpile and contain the material on airport property as shown on sheet J.03 and U.03. All labor, equipment, and material necessary to identify, isolate, haul, stockpile, cover, and contain the contaminated soil shall be included in the cost of this item.
 - **REVISE** Item 11 Reference Note C to “The owner shall employ and pay for an environmental testing agency (Terracon) to perform on-site photoionization testing (PID) of soils that the contractor perceives to be contaminated. Upon notification, Terracon shall assist in identifying contaminated soils that shall be isolated and hauled to the airport stockpile location. Contractor coordination with RPR and Terracon is a requirement of this bid item.
- **Plan Sheet J.03 – Construction Safety and Phasing Plan – Overall Safety and Phasing Plan**
 - **ADD** Note “Contaminated Soils Stockpile Location. Refer to Sheets C.02 and U.03 for Details.”
- **Plan Sheet L.01 and L.02 – Geometrics, Staking, and Jointing Plan**
 - **REVISE** Referenced Note 2 to “Storm Sewer Intake. Type A-2 Joint Required Around All Intakes. Refer to M-Sheets for Locations and Details.”
 - **REVISE** “A-2 = 1 Inch Isolation Joint at Hangar” to “A-2 = 1 Inch Isolation Joint”.
- **Plan Sheet M.06 – Storm Sewer and Subdrain – Storm Intake Details**
 - **ADD** Referenced Note 3 “Type “A-2” – 1 Inch Isolation Joint. Refer to B-Sheets and L-Sheets for Details.”
- **Plan Sheet U.03 – Storm Water Pollution Prevention Plan (SWPPP) – Contractor Staging/Batch**

Dubuque Regional Airport
Dubuque, Iowa

FAA AIP No.: 3-19-0028-081
IA DOT No.: 9I240DBQ100

Plant

- **ADD** Note “Contaminated Soils Stockpile Location.
- **ADD** Referenced Note 1 “Surrounding Stockpile with Silt Fence and Tarping per the SGMP Shall be Incidental to the *Remove and Stockpile Contaminated Soil* Bid Item. Contractor Coordination with the RPR and Environmental Engineer is a Requirement of this Bid Item.”

Plan/Specification Attachments

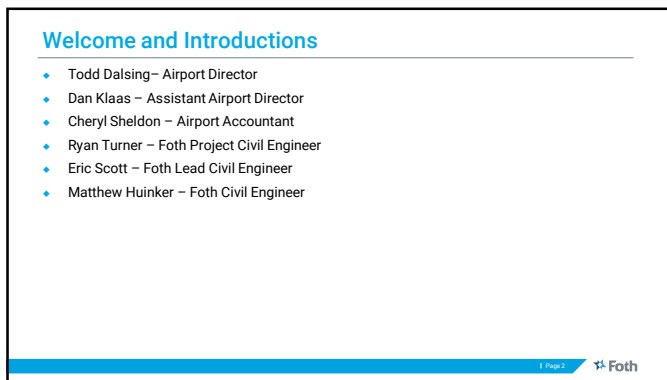
- **Specifications:** Proposal Form (Pages 1 and 2 only).
- **Drawings:** B.02, C.01, C.02, L.01, L.02, M.06, U.03

END OF ADDENDUM



1

Foth utilized these power point slides to facilitate the pre-bid meeting. In general, the meeting followed the material presented in the slides. Additional information provided will serve as meeting minutes for those whom were unable to attend.



2

Ryan Turner - Main Contact During Bidding

Dan Klaas - Contact for site visits. 563-589-4232, or dklaas@cityofdubuque.org



3

Utilized google earth to show the project location off of Airport road near the Dubuque Jet Center and the large corporate box hangars.

The project has both Federal (FAA) and State of Iowa (AIP) funding grants. There are special provisions requirements for each funding source. In general, the project utilizes FAA technical specifications.

Scope of Project

- Unclassified Excavation – Excess cut material (property of the contractor)
 - Soil and Groundwater Management Plan
- Pavement Removals
- Utility Removal
 - Storm sewer pipe and structures
 - Abandoned fueling hydrants
 - Abandoned electrical duct bank
- RCP Storm Sewer, Subdrain, and Storm Sewer Intakes
 - Permanent Storm Intake Pollution Prevention Device (Safe Drain or approved equal)
- Pavement Section
 - P-501 PCC
 - P-219 Recycled Concrete Aggregate Base Course
 - P-154 Subbase Course
- Chamfered joints and compression seals
- Pavement Markings

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Soil and Groundwater Management Plan - Environmental monitoring during geotechnical investigation show that encountering contaminated soils during excavations are likely. Contractors shall refer to the SGMP on appropriate means of handling, isolation, hauling, and disposal of contaminated soils. *Not discussed during the meeting, but the requirements of disposing of contaminated soils off site at the landfill prescribed in the SGMP has been modified in this addendum. See changes to the estimate reference notes.*

Reinforced Concrete Pipe (RCP) has been specified as the storm sewer pipe material. A substitution request for Polypropylene pipe has been received, however, the engineer does not plan to accept this request; pipe material shall remain as RCP as shown in the bid documents and the D-701 specification.

Bidding Information

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Bidding Schedule

- ◆ Pre-Bid Meeting – Wednesday, March 6 at 10:00 am
- ◆ Last Day for Questions – Thursday, March 14 at 5:00 pm
- ◆ Bids Due – Thursday, March 21 at 1:00 pm.
- ◆ Owner may hold the awarded bid up to 120 calendar days from date of receipt of bids before executing the contract.

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Important to note:

Bids may be held by the Dubuque Regional Airport Commission for a period not to exceed **30 days** from the date of the bid opening for the purpose of evaluating bids prior to award of contract. Award of contract will be contingent on receipt of Federal funding, and the awarded Bid may be held for **120 days** before a contract is signed. The Dubuque Regional Airport Commission reserves the right to reject any or all bids or to waive any informalities or minor irregularities or to re-advertise for bids.

Bidding Requirements

- All included in Specification Manual*
- Standard Form of Bid (unit price contract) – pg 29
- Acknowledgement of Addenda – pg 32
- Signed Proposal Form – pg 34
- Identity of Subcontractors – pg 37
- DBE Utilization Statement (1.50% goal) – pg 39
- DBE Letter of Intent (one for each DBE firm) – pg 41
 - *Good Faith Efforts must be submitted if not able to achieve the project goal.*
- Targeted Small Business (TSB) – Pre-Bid Contact Information Form – pg 43
- Buy American Certification – pg 43
 - *Waiver requests shall be submitted after the bid if not able to meet 100%.*
- Bid Bond (Separate Envelope) – pg 45

* Page Numbers Refer to PDF of the Manual

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All requirements and attachments of the proposal form are due at time of bid.

There is a DBE goal of 1.50% - joint sealant, traffic control, pavement markings, trucking, subdrain, etc. are all good opportunities. Need to exhibit a good faith effort if project goal is not met.

The project also has TSB requirements - no goal requirement. Note that TSB and DBE are not the same! Not all TSB companies are DBE companies and Vice Versa - make sure you're soliciting companies on the current TSB and current DBE lists. Good faith efforts and completion of the required forms are necessary to have a responsive bid - pre-bid contact with DBE and TSB should begin ASAP.

Note the Buy American Certification Form.

Bid Bond shall accompany the bid and provided in a separate envelope from the other proposal documents.

DBE Information

- Only DBE's currently on the Iowa DOT DBE Directory qualify
- From Supplementary Provision A – Federal Provisions – pg 146
- Good Faith Efforts if not meeting DBE goal of 1.50%
 - Offering assistance in areas of interpreting plans and preparing proposals;
 - Response to requests from any small businesses submitting quotes, concerning quantities, overtime, project scheduling, etc.;
 - Making portions of the work available for DBE subcontracting;
 - Evidence of negotiations with DBE firms;
 - DBE quotes obtained and non-DBE quotes used;
 - Reason agreements were not reached;
 - Follow-up after initial solicitations;
 - Efforts to assist in obtaining bonding, lines of credit, or insurance; and
 - Efforts to assist in obtaining equipment, supplies, and materials.
- *Pro forma mailings to DBEs requesting bids are not alone sufficient to satisfy good faith efforts.*
- Counting DBE Participation
 - DBE Manufacturer, Truck, Contractor = 100%
 - DBE Supplier = 60%

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Davis Bacon Wage Rates

- Required for all contractors / subcontractors working on-site.
- Weekly Certified Payroll's throughout construction.
- Page 153 of project manual

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Current wage determinations have been included within the specification manual.

TSB Information

- TSB and DBE are not the same, and both are required!!!
- Only TSBs currently on the Iowa Economic Development Authority Directory qualify
 - See iowaeda.com directory.
- From Supplementary Provision, Attachment 5 – pg 245
- No goal assigned but positive effort documentation required.

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Construction Schedule

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Construction Schedule

- Award of Contract anticipated mid July 2024
- Calendar Day and Completion Date Contract
 - Phase 1 – Early Start Date: July 24, Late Start Date: August 14.
 - 90-calendar day completion
 - Phase 1A – 3 calendar days
 - Phase 1C – 7 calendar days
 - Phase 2 – 1 Calendar day
 - Phase 3 – 1 Calendar Day

SA LARSON RECONSTRUCTION SCHEDULE

Schedule	Liquidated Damages Cost	Allowed Construction Time
Phase 1	\$500 per Calendar Day	90 Calendar Days
Total Project Completion	\$100 per Calendar Day	November 14, 2024

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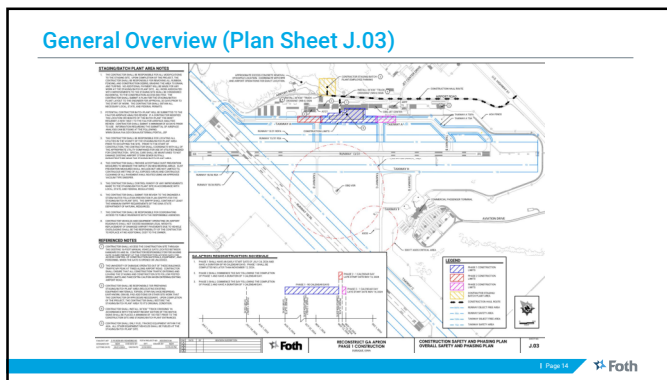
See J-sheets for traffic control requirements for the project. Contractor shall supply all traffic control, signage, etc. required by the plan and spec. documents.

Phases 1A and 1C are consecutive calendar days. The intent is that all work within the Taxilane A object free area will be completed within either Phase 1A or Phase 1C. *Not discussed during the meeting, but joint sealing requires concrete curing of at least seven days. The Q&A log in addendum 1 will indicate that pavement markings and joint sealing within the taxilane object free area may be completed outside of the Phase 1C calendar day requirement; coordination with the Airport required 72 hours in advance.*

Refer to the sample form of contract in the specification manual for liquidated damages if Phase 1 construction extends beyond 90-calendar days or if project completion extends beyond November 14, 2024.



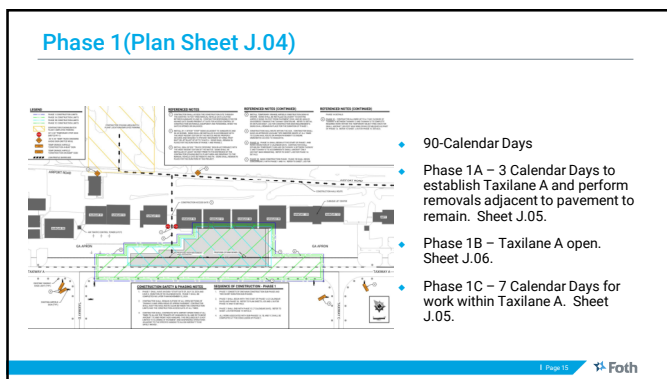
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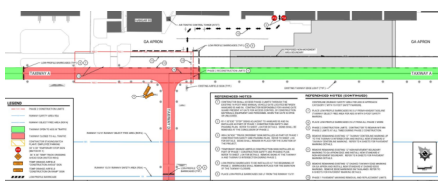
See J-sheets and CSPP for construction safety and phasing requirements!

Coordination Airport, nearby tenants, and Air Traffic control required at all times.



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Phase 2 (Plan Sheet J.07)

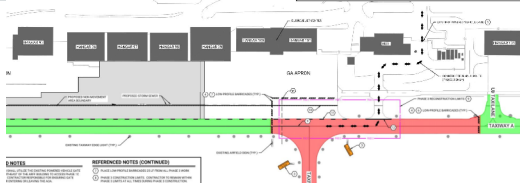


- Taxiway D Closure. Taxiway A closure from T-hangers to Taxiway D.
- Pavement Marking Installation

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Phase 3 (Plan Sheet J.08)



- Taxiway C Closure. Taxiway A closure from Taxiway C to UD Taxilane.
- Pavement Marking Installation

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Airport Safety and Security
Considerations

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Airport Safety and Security

- Contractor will be required to Obtain Badges by completing Security Threat Assessment.
- Contractor shall provide all traffic control as shown on the plans.
- DBQ Operations Staff to escort contractor into the Secured AOA and Open Pavement on the Airfield during placement of traffic control and closure of pavement. Once traffic control is in place and taxiway is closed the Contractor will be allowed unescorted access within the work area.
- Coordination with Airport.
- Airport Security Requirements – See Attachment 4 of the Supplementary Provisions.
- Construction Safety and Phasing Plan – See Attachment 3 of the Supplementary Provisions.
- Awarded Contractor to Submit Safety Plan Compliance Document

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Security Badges will be required. Badge holders will be required to go to a training class (approx. 1-2 hours). The badge fee is stated in the bidding documents as \$60/badge. Badge holders can receive escort training so that they can escort other members of their team - individuals being escorted shall be within sight and sound of the person performing the escort.

Project Specific Design and Construction Information

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Design Components – Plans and Spec.

- FAA standard specifications.
- Approx. 6,500 CY of Unclassified Excavation (Excess Cut Material)
 - Soil and Groundwater Management Plan
- Pavement Section – 11.5" P-501, 6" P-219, 10" P-154.
- Storm Sewer Installation – Reinforced Concrete Pipe.
- Storm Sewer Intakes
 - Over-excavation for Bedding Material.
 - Aircraft Rated Frame and Casting.
 - Permanent storm intake pollution prevention device.
- Contractor Quality Control Program – C-100 specification
 - Pay Item – Quality is more than test results.
 - QC/QA workshop prior to construction. Separate from preconstruction meeting.

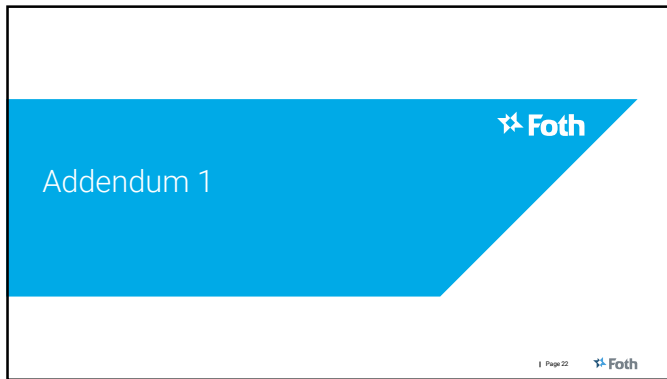
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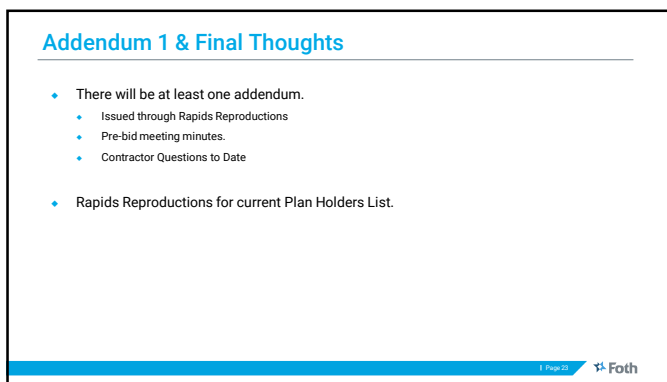
FAA standard specifications for all work, including the portion of work that's funded by the Iowa DOT.

Contaminated soils are anticipated. A bid item has been created to establish a unit price for managing and disposing of contaminated soils as encountered *Not discussed during the meeting, but notice the changes to this contaminated soils bid item and estimate reference notes issued in Addendum 1.*

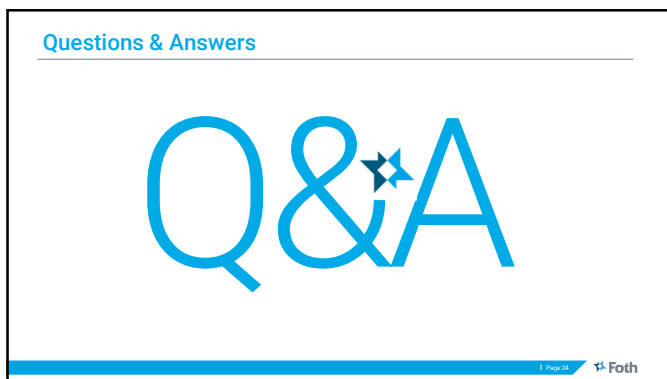
DBQ fuels aircraft on this apron - notice the bid item for permanent storm sewer intake protection devices.



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Refer to the question and answer log provided within Addendum 1. The question and answer log includes the two questions asked during the meeting.

Attendee List

Turner, Ryan P	Ryan.Turner@foth.com
Charles Arnold	cjarnold@pirtobin.com
mike portzen (Guest)	Portzen Construction
Steve Horsfield. Connolly Construction Inc.	Connolly Construction
Dan Klaas	dklaas@cityofdubuque.org
Cheryl Sheldon	CSheldon@cityofdubuque.org
Kevin (Guest)	
Dawn King	dawnk@boomerangcorp.com
Matt Schmitt	Flynn Company
Matt Gansen	Gansen Excavating
Huinker, Matthew	Matthew.Huinker@foth.com
Bob (Guest)	Gansen Excavating
15632277010	
Todd Dalsing	tdalsing@cityofdubuque.org

No:	Plan / Spec Item	Question	Response
ADDENDUM #1 QUESTIONS			
1.	Spec D-701 – Pipe for Storm Drains and Culverts	Is polypropylene storm sewer pipe an acceptable material for use on this project.	<i>RCP storm pipe material will be a requirement for this project. Due to how shallow the proposed storm sewer is with future aircraft loads, along with the existing storm material being RCP, we have intentionally only specified RCP within the M-Sheets and the D-701 Specification.</i>
2	Sheet D.01	Plan sheet D.01 calls for the removal of 43 LF of abandoned storm drain. What bid item will this be paid under?	<i>All drainage pipe removal will be paid under Bid Item 4 – Drainage Pipe Removal, Less Than 18 Inch Dia.</i>
3	Spec P-501 – Cement Concrete Pavement	Can equipment be driven on base during paving operations.	<i>Equipment may not be driven on the base of any current paving lane. Equipment may be driven on lanes adjacent to the lane being paved but any damage to the base material shall be corrected at the cost of the contractor. Refer to specification section P-501-4.4 for base surface preparation prior to placement requirements.</i>
4	Spec P-501 – Cement Concrete Pavement	Can concrete be placed directly off a ready-mix truck.	<i>Concrete shall be placed directly in front of the paver from outside the paving lane and shall be spread evenly across the paving lane to a depth which permits the paver to operate efficiently as specified in specification P-501-4.2a. Refer to Specification P-501-4.8 for additional concrete placement details.</i>
5	Spec P-501 – Cement Concrete Pavement	Does P-501 have to be produced on airport property?	<i>P-501 Cement Concrete pavement does not need to be batched onsite. If the contractor elects to batch concrete offsite they shall ensure that all requirements of the P-501 specification including maximum haul time limitations.</i>
6	Spec P-219 – Recycled Concrete Aggregate Base Course	Does P-219 have to be crushed on airport property?	<i>P-219 Recycled Concrete Aggregate Base Course does not need to be produced on site. If the contractor elects to crush offsite they shall ensure uninhibited access to the crushing site by the RPR. Regardless of the crushing site, all material utilized to produce P-219 shall come from the project site and all requirements of the P-219 specification shall be met.</i>
7	Instructions to Bidders	Can you clarify the full mailing address where bids are to be submitted.	<i>Refer to the first page of the proposal document for the address at which proposals shall be received. Office of the Airport director - 11000 Airport Road, Dubuque, IA 52003.</i>
8	Instructions to Bidders	What section of the bid form do Limited Liability Companies fill out? (only Individual, Partnership, Corporation and Joint Venture are listed as options)	<i>LLCs have typically signed under corporation. When completing and signing the proposal form indicate that the company is an LLC.</i>
9	Instructions to Bidders	How does article 2.01 of the Instructions to Bidders apply to being a qualified bidder?	<i>The project does not require considerable manufactured equipment, the engineer/owner will not require any qualification statements for proposed manufactured equipment during bidding.</i>

10	Spec D-705 – Pipe Underdrains for Airports	What are material requirements for subdrain pipe?	<i>Subdrain pipe material shall meet the requirements of specification section D-705-2.2.</i>
11	CSPP & J-sheets	Is Phase 1C seven consecutive calendar days? We will need seven days of pavement cure time at a minimum prior to installing joint seals.	<i>PCC joint sealing and pavement markings within the Temporary Taxilane A object free area (TOFA) may occur outside of the Phase 1C seven calendar day closure window; 72-hour advanced notification to the RPR and Airport required before reclosing Taxiway A to complete this work.</i>
12	Sheet B.02	Can the engineer specify the joint width for contraction and construction joints? The compression joint supplier will only make a recommendation on the size of compression seal based on a width of joint given to them in the plans.	<i>The plan sheet will be reissued with an update to details 7 and 8 on sheet B.02. Contraction and Construction joint width shall be ½”.</i>

PROPOSAL FORM

AIP Project: 3-19-0028-081
Iowa DOT Project: 9I240DBQ100

TO: The Dubuque Airport Commission
11000 Airport Road
Dubuque, Iowa 52003

The undersigned, in compliance with the request for bids for construction of the following Project:

Reconstruct GA Apron – Phase 1 Construction

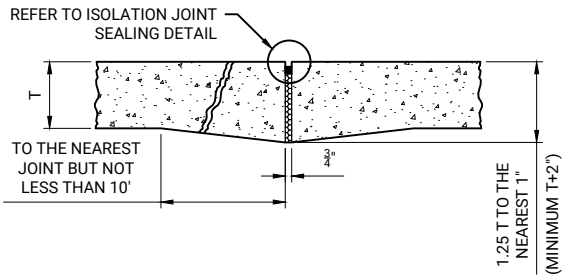
hereby proposes to furnish all labor, permits, material, machinery, tools, supplies and equipment to faithfully perform all work required for construction of the Project in accordance with the project manual, project drawings and issued Addenda within the specified time of performance for the following prices:

ITEM NO.	ITEM	QUANTITY	UNIT	UNIT PRICE	EXTENSION
1	AIRFIELD PAVEMENT REMOVAL, VARIABLE DEPTH HMA (2.5 - 3.5 INCH) ON PCC (7.5-10.5 INCH)	SY	12,025		
2	AIRFIELD PAVEMENT MARKING REMOVALS	SF	8,700		
3	FULL DEPTH SAW CUTTING	LF	1,850		
4	DRAINAGE PIPE REMOVAL, LESS THAN 18 INCH DIA.	LF	730		
5	DRAINAGE STRUCTURE REMOVAL	EA	4		
6	REMOVE AND DISPOSE OF ABANDONED 6 INCH DIP FUEL SYSTEM PIPE	LF	175		
7	REMOVE AND DISPOSE OF ABANDONED FUEL SYSTEM STRUCTURE AND INFRASTRUCTURE	EA	2		
8	REMOVE AND DISPOSE OF ABANDONED 2-WAY 4 INCH CONCRETE ENCASED DUCT BANK	LF	95		
9	REMOVE AND DISPOSE OF ABANDONED 8-WAY 4 INCH CONCRETE ENCASED DUCT BANK	LF	95		
10	UNCLASSIFIED EXCAVATION	CY	6,393		
11	REMOVE AND STOCKPILE CONTAMINATED SOIL	CY	500		
12	GEOGRID	SY	12,025		
13	SUBBASE COURSE, 10 INCH	SY	12,025		
14	RECYCLED CONCRETE AGGREGATE BASE COURSE, 6 INCH	SY	12,025		
15	11.5 INCH PCC PAVEMENT	SY	12,025		
16	COMPRESSION JOINT SEALS	LF	10,660		
17	JOINT SEALING FILLER	LF	2,050		
18	PAVEMENT MARKINGS - TEMPORARY 6" YELLOW CENTERLINE MARKING	SF	420		

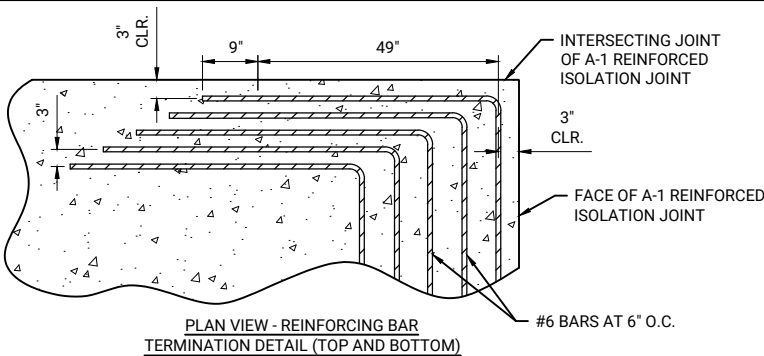
19	PAVEMENT MARKINGS - YELLOW TAXIWAY PAINT, TYPE 1 BEADS	SF	740		
20	PAVEMENT MARKINGS - YELLOW TAXIWAY PAINT, TYPE 3 BEADS	SF	1,890		
21	PAVEMENT MARKINGS - BLACK OUTLINES, PAINT	SF	6,230		
22	15 INCH RCP, CLASS V, GASKETED	LF	690		
23	PERFORATED SUBDRAIN, 6 INCH DIA.	LF	2,075		
24	SUBDRAIN CLEANOUT	EA	10		
25	MODIFIED SW-511 SINGLE GRATE INTAKE	EA	4		
26	SW-211, STORM SEWER CONNECTION TO EXISTING PIPE	EA	1		
27	PERMANENT STORM INTAKE POLLUTION PREVENTION DEVICE IN MODIFIED SW-511 SINGLE GRATE INTAKE	EA	4		
28	CONTRACTOR QUALITY CONTROL PROGRAM	LS	1		
29	STORM WATER POLLUTION PREVENTION PLAN (SWPPP), PREPARATION AND MANAGEMENT	LS	1		
30	TEMPORARY INLET PROTECTION DEVICE	EA	4		
31	MOBILIZATION AND MISCELLANEOUS	LS	1		
32	TRAFFIC CONTROL AND SECURITY	LS	1		
33	CONSTRUCTION ACCESS	LS	1		
34	CONSTRUCTION SURVEY	LS	1		

COMPUTED TOTAL BASE BID (ITEMS 1 THROUGH 34)

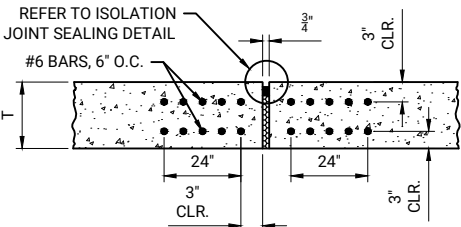
(NUMBERS)



01 TYPE "A" - THICKENED EDGE ISOLATION JOINT
SCALE: NONE

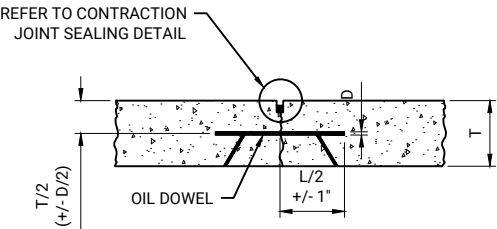


02 TYPE "A-1" - REINFORCED ISOLATION JOINT
SCALE: NONE

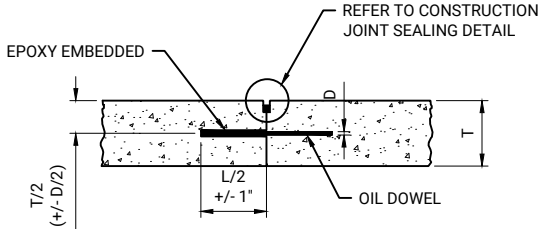


- NOTES**
- REINFORCING BARS SHALL NOT CONTINUE THROUGH ANY JOINT LINE. THE #6 BARS SHOULD END 3 INCHES FROM ANY JOINT LINE.
 - AT EACH INTERSECTING JOINT, BOTH TOP AND BOTTOM REINFORCING BARS SHOULD BE BENT 90 DEGREES IN THE HORIZONTAL PLANE AND CONTINUE AT LEAST 9 INCHES BEYOND A POINT LOCATED A DISTANCE OF 49 INCHES FROM THE FACE OF THE ISOLATION JOINT.

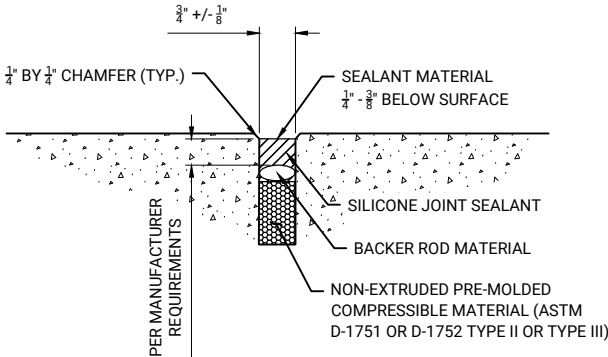
03 TYPE "A-2" - 1 INCH ISOLATION JOINT
SCALE: NONE



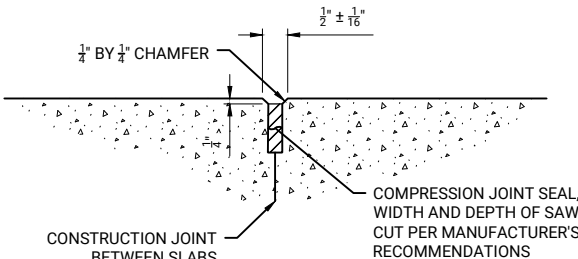
04 TYPE "C" - DOWELED CONTRACTION JOINT
SCALE: NONE



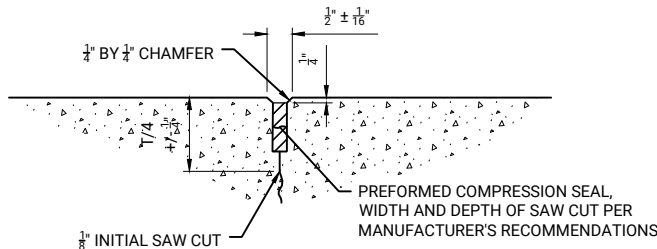
05 TYPE "E" - DOWELED CONSTRUCTION JOINT
SCALE: NONE



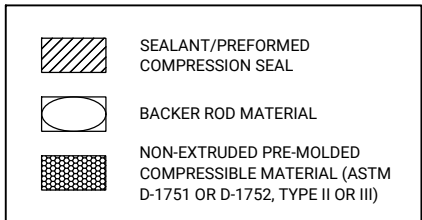
06 ISOLATION JOINT SEALING DETAIL
SCALE: NONE



07 CONSTRUCTION JOINT SEALING DETAIL
SCALE: NONE



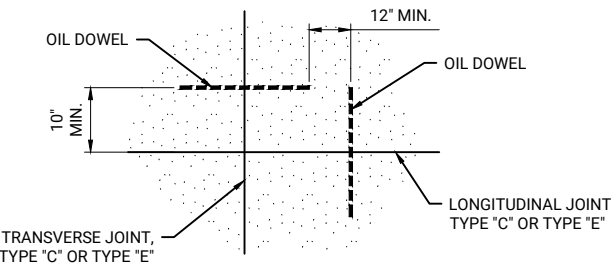
08 CONTRACTION JOINT SEALING DETAIL
SCALE: NONE



09 JOINT DETAIL LEGEND
SCALE: NONE

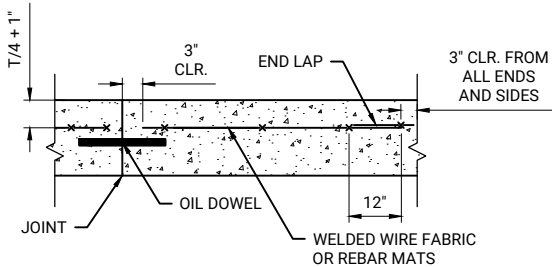
- NOTES**
- ELIMINATE DOWEL, OR TIE BAR, FROM LONGITUDINAL JOINT AS NECESSARY TO MAINTAIN 12 INCHES FROM THE END OF ALL TRANSVERSE DOWEL BARS.

10 POSITION OF DOWELS AT EDGE OF JOINT, TYPE "C" AND TYPE "E"
SCALE: NONE



THICKNESS OF SLAB	DIAMETER OF DOWEL	LENGTH	SPACING
7.5-12 IN.	1 IN.	18 IN.	12 IN.

11 DIMENSIONS AND SPACING OF STEEL DOWELS
SCALE: NONE



12 REINFORCED CONCRETE PANEL DETAIL
SCALE: NONE

- NOTES**
- REINFORCEMENT SHALL BE CONTINUOUS THROUGHOUT THE PANEL. ALL REINFORCING SHALL BE EPOXY COATED. IF WELDED WIRE FABRIC IS USED IT SHALL BE SUPPLIED IN SHEETS, NOT ROLLS.

REINFORCING MAY CONSIST OF THE FOLLOWING:
 - #3 DEFORMED REBAR AT 12" SPACING (MAX.)
 - W6 6X6 WELDED WIRE FABRIC
 - MINIMUM REINFORCING REQUIRED IS 0.069 SQ IN/LF. CONTRACTOR MAY SUBSTITUTE A DIFFERENT REINFORCING SCHEME WITH PRIOR APPROVAL BY THE ENGINEER.
 - ALL PANELS WITH A LENGTH TO WIDTH (L/W) RATIO EXCEEDING 1.25 SHALL BE REINFORCED.

- NOTES**
- DOWELS SHALL BE FULLY SHOP PAINTED WITH CORROSION INHIBITING EPOXY COATING CONFORMING TO ASTM A1078 PRIOR TO DELIVERY. DOWELS WITH DAMAGED EPOXY COATING MAY BE REJECTED AT THE DISCRETION OF THE ENGINEER OR RPR.
 - INITIAL CONTRACTION JOINT SAW CUTS SHALL BE MADE AS SOON AS PRACTICAL WITHOUT DAMAGING THE PAVEMENT SURFACE. JOINT CONSTRUCTION SHALL FOLLOW THE REQUIREMENTS OF ACI 325.9R-15.
 - DIMENSIONS AND DEPTH OF JOINTS ARE TYPICAL BASED ON BEST PRACTICE. CONTRACTOR SHALL ADVISE THE ENGINEER OR RPR ON DIMENSIONS IF PROPOSED MATERIALS RECOMMEND MODIFICATION.

13 JOINTING NOTES
SCALE: NONE

ESTIMATED PROJECT QUANTITIES

ITEM NO.	ITEM CODE	DESCRIPTION	UNIT	QUANTITIES					
				ESTIMATED			AS-BUILT		
				DIVISION 1	DIVISION 2	TOTAL	DIVISION 1	DIVISION 2	TOTAL
1	P-101	AIRFIELD PAVEMENT REMOVAL, VARIABLE DEPTH HMA (2.5 - 3.5 INCH) ON PCC (7.5-10.5 INCH)	SY	9,985	2040	12,025			
2	P-101	AIRFIELD PAVEMENT MARKING REMOVALS	SF	8,700	0	8,700			
3	P-101	FULL DEPTH SAW CUTTING	LF	1,375	475	1,850			
4	P-101	DRAINAGE PIPE REMOVAL, LESS THAN 18 INCH DIA.	LF	730	0	730			
5	P-101	DRAINAGE STRUCTURE REMOVAL	EA	4	0	4			
6	P-101	REMOVE AND DISPOSE OF ABANDONED 6 INCH DIP FUEL SYSTEM PIPE	LF	175	0	175			
7	P-101	REMOVE AND DISPOSE OF ABANDONED FUEL SYSTEM STRUCTURE AND INFRASTRUCTURE	EA	2	0	2			
8	P-101	REMOVE AND DISPOSE OF ABANDONED 2-WAY 4 INCH CONCRETE ENCASED DUCT BANK	LF	95	0	95			
9	P-101	REMOVE AND DISPOSE OF ABANDONED 8-WAY 4 INCH CONCRETE ENCASED DUCT BANK	LF	95	0	95			
10	P-152	UNCLASSIFIED EXCAVATION	CY	5,408	985	6,393			
11	SPECIAL	REMOVE AND STOCKPILE CONTAMINATED SOIL	CY	0	500	500			
12	P-154	GEOGRID	SY	9,985	2040	12,025			
13	P-154	SUBBASE COURSE, 10 INCH	SY	9,985	2040	12,025			
14	P-219	RECYCLED CONCRETE AGGREGATE BASE COURSE, 6 INCH	SY	9,985	2040	12,025			
15	P-501	11.5 INCH PCC PAVEMENT	SY	9,985	2040	12,025			
16	P-604	COMPRESSION JOINT SEALS	LF	9,010	1650	10,660			
17	P-605	JOINT SEALING FILLER	LF	1,530	520	2,050			
18	P-620	PAVEMENT MARKINGS - TEMPORARY 6" YELLOW CENTERLINE MARKING	SF	420	0	420			
19	P-620	PAVEMENT MARKINGS - YELLOW TAXIWAY PAINT, TYPE 1 BEADS	SF	740	0	740			
20	P-620	PAVEMENT MARKINGS - YELLOW TAXIWAY PAINT, TYPE 3 BEADS	SF	1,890	0	1,890			
21	P-620	PAVEMENT MARKINGS - BLACK OUTLINES, PAINT	SF	6,230	0	6,230			
22	D-701	15 INCH RCP, CLASS V, GASKETED	LF	690	0	690			
23	D-705	PERFORATED SUBDRAIN, 6 INCH DIA.	LF	2,075	0	2,075			
24	D-705	SUBDRAIN CLEANOUT	EA	10	0	10			
25	D-751	MODIFIED SW-511 SINGLE GRATE INTAKE	EA	4	0	4			
26	D-751	SW-211, STORM SEWER CONNECTION TO EXISTING PIPE	EA	1	0	1			
27	SPECIAL	PERMANENT STORM INTAKE POLLUTION PREVENTION DEVICE IN MODIFIED SW-511 SINGLE GRATE INTAKE	EA	4	0	4			
28	C-100	CONTRACTOR QUALITY CONTROL PROGRAM	LS	1	0	1			
29	C-102	STORM WATER POLLUTION PREVENTION PLAN (SWPPP), PREPARATION AND MANAGEMENT	LS	1	0	1			
30	C-102	TEMPORARY INLET PROTECTION DEVICE	EA	4	0	4			
31	C-105	MOBILIZATION AND MISCELLANEOUS	LS	0.85	0.15	1			
32	C-105	TRAFFIC CONTROL AND SECURITY	LS	0.85	0.15	1			
33	C-105	CONSTRUCTION ACCESS	LS	0.85	0.15	1			
34	GP-50	CONSTRUCTION SURVEY	LS	0.85	0.15	1			



TABULATION OF EXISTING CONCRETE MATERIAL AVAILABLE FOR THE PROPOSED P-219 RECYCLED CONCRETE AGGREGATE BASE COURSE		
LOCATION	EXISTING CONCRETE PAVEMENT (CY)	COMMENTS
GA APRON STA. 842+96.75 TO STA. 850+14.25	3,195 CY (9 INCH DEPTH)	EXISTING CONCRETE DEPTH VARRIES FROM 7.5" - 10.5" ACROSS GA APRON

TABULATION OF EXISTING HMA MILLING MATERIAL AVAILABLE FOR THE PROPOSED P-219 RECYCLED CONCRETE AGGREGATE BASE COURSE		
LOCATION	EXISTING ASPHALT PAVEMENT (CY)	COMMENTS
GA APRON STA. 842+96.75 TO STA. 850+14.25	1,065 CY (3 INCH DEPTH)	EXISTING HMA DEPTH VARRIES FROM 2.5" - 3.5" ACROSS GA APRON

TABULATION OF PROPOSED P-219 RECYCLED CONCRETE AGGREGATE BASE COURSE FOR GA APRON PAVEMENT		
LOCATION	PROPOSED P-219 REQUIRED (CY)	COMMENTS
GA APRON STA. 842+96.75 TO STA. 850+14.25	2,130 CY (6 INCH DEPTH)	HMA MILLINGS CAN BE USED UP TO 10% OF THE PROPOSED P-219 RECYCLED PCC AGGREGATE BASE COURSE

NOTES

- SWELL/WASTE FACTOR NOT INCLUDED IN QUANTITIES. CALCULATIONS BASED ON ACTUAL MATERIAL IN-PLACE.
- MATERIAL AVAILABLE IS AN ESTIMATE ONLY BASED ON PAVEMENT CORES AND EXISTING AS-BUILT DRAWING INFORMATION. ACTUAL MATERIAL AVAILABLE WILL VARY.
- EXISTING MATERIAL MAY ONLY BE USED IF IT MEETS THE P-219 SPECIFICATION.
- ASPHALT MILLINGS MAY BE USED IN THE P-219 RECYCLED AGGREGATE BASE COURSE (LIMITED TO 10% OF THE P-219 MIX).
- ANY LEFTOVER CRUSHED OR UNCRUSHED CONCRETE SHALL BECOME PROPERTY OF THE AIRPORT AND SHALL BE DELIVERED TO THE STOCKPILE SITE SHOWN ON THE J-SHEETS (CONTRACTOR SHALL COORDINATE WITH RPR AND AIRPORT PRIOR TO STOCKPILING MATERIAL).

NO	DATE	BY	REVISION DESCRIPTION
1	03/11/24	MDH	ADDENDUM NO. 1
2			
3			
4			



ESTIMATE REFERENCE INFORMATION		
ITEM NO.	ITEM CODE	DESCRIPTION
1	P-101	AIRFIELD PAVEMENT REMOVAL, VARIABLE DEPTH HMA (2.5-3.5 INCH) ON PCC (7.5-10.5 INCH) A. THIS ITEM INCLUDES THE REMOVAL OF EXISTING ASPHALT PAVEMENT, CONCRETE PAVEMENT, AND AGGREGATE BASE. REMOVALS SHALL OCCUR TO THE BOTTOM OF THE EXISTING AGGREGATE BASE. B. CONTRACTOR SHALL UTILIZE ASPHALT MILLINGS GENERATED FROM PAVEMENT REMOVAL FOR THE PRODUCTION OF P-219 RECYCLED CONCRETE AGGREGATE BASE COURSE (BID ITEM 14). REFER TO THE P-219 SPECIFICATION FOR ALLOWABLE MILLINGS IN P-219. EXCESS ASPHALT MILLINGS SHALL BE HAULED TO THE LOCATIONS SHOWN ON THE J-SHEETS AND BECOME PROPERTY OF THE OWNER. THE HAULING AND STOCKPILING OF REMAINING ASPHALT MILLINGS SHALL BE INCIDENTAL TO THE PAVEMENT REMOVAL BID ITEM. C. CONTRACTOR SHALL UTILIZE PCC PAVEMENT REMOVALS FOR PRODUCTION OF P-219 RECYCLED CONCRETE AGGREGATE BASE COURSE (BID ITEM 14). CONTRACTOR SHALL HAUL AND STOCKPILE ALL UNUTILIZED CONCRETE FOR THE AIRPORT TO USE ON FUTURE AIRFIELD PROJECTS. THE STOCKPILE LOCATION FOR THIS UNCRUSHED CONCRETE IS SHOWN ON THE J-SHEETS. THE HAULING AND STOCKPILING OF UNCRUSHED CONCRETE SHALL BE INCIDENTAL TO THIS BID ITEM.
2	P-101	AIRFIELD PAVEMENT MARKING REMOVALS A. ALL PAVEMENT MARKING REMOVALS ON ASPHALT PAVEMENT SHALL BE ACCOMPLISHED UTILIZING COLD MILLING OR ROTARY GRINDING. B. ALL PAVEMENT MARKING REMOVALS ON PCC PAVEMENT SHALL BE ACCOMPLISHED UTILIZING HIGH PRESSURE WATER BLASTER. C. REMOVAL METHODS SHALL NOT CAUSE MAJOR DAMAGE TO PAVEMENT OR TO ANY STRUCTURE ADJACENT TO THE WORK AREA. REFER TO SPECIFICATION SECTION P-101-3.3 FOR WHAT IS CONSIDERED "MAJOR DAMAGE". D. CONTRACTOR SHALL PERFORM A TEST STRIP OF EACH REMOVAL METHOD ON EACH PAVEMENT TYPE AND OBTAIN APPROVAL BY THE RPR PRIOR TO IMPLEMENTING FULL SCALE OPERATIONS. E. THE CLEANING AND REMOVAL OF ALL DEBRIS CREATED DURING PAVEMENT MARKING REMOVAL SHALL BE INCIDENTAL TO THE AIRFIELD PAVEMENT MARKING REMOVAL BID ITEM. F. REFER TO SHEETS K.01 AND K.02 FOR AIRFIELD PAVEMENT MARKING REMOVAL LOCATIONS AND SHEET K.05 FOR TABULATIONS.
3	P-101	FULL DEPTH SAW CUTTING A. DOUBLE SAW CUT ON THE EXISTING PAVEMENT SHALL BE INCIDENTAL TO THIS BID ITEM. REFER B-SHEETS FOR FULL DEPTH SAW CUTTING DETAILS AND D-SHEETS FOR SAW CUT LOCATIONS.
4	P-101	DRAINAGE PIPE REMOVAL, LESS THAN 18 INCH DIA. DRAINAGE STRUCTURE REMOVAL A. REFER TO D-SHEETS FOR DRAINAGE PIPE AND STORM STRUCTURE REMOVAL LOCATIONS AND DETAILS. B. REMOVALS SHALL BECOME THE PROPERTY OF THE CONTRACTOR, NO DISPOSAL SITE EXISTS ON THE AIRPORT. C. THE BACKFILL FOR PIPE TRENCHES AND STRUCTURES SHALL MEET THE P-152 SPECIFICATION AND SHALL BE INCIDENTAL TO THIS BID ITEM. D. SALVAGING EXISTING SAFE DRAIN FUEL INSERTS TO THE AIRPORT SHALL BE INCIDENTAL TO THE DRAINAGE STRUCTURE REMOVAL BID ITEM.
5	P-101	
6	P-101	REMOVE AND DISPOSE OF ABANDONED 6 INCH DIP FUEL SYSTEM PIPE REMOVE AND DISPOSE OF ABANDONED FUEL SYSTEM STRUCTURE AND INFRASTRUCTURE A. REFER TO D-SHEETS FOR ABANDONED 6 INCH DIP FUEL SYSTEM PIPE AND ABANDONED FUEL SYSTEM STRUCTURE LOCATIONS AND DETAILS. B. CARPING 6 INCH DIP FUEL SYSTEM PIPE AT REMOVAL LIMITS SHALL BE INCIDENTAL TO THE ABANDONED 6 INCH DIP FUEL SYSTEM PIPE REMOVAL BID ITEM. C. REMOVALS SHALL BECOME THE PROPERTY OF THE CONTRACTOR, NO DISPOSAL SITE EXISTS ON THE AIRPORT. CONTRACTOR SHALL DISPOSE OF ALL MATERIALS IN ACCORDANCE WITH ALL FEDERAL, STATE AND LOCAL LAWS AND REGULATIONS, AND PER THE RECOMMENDATIONS OF THE SOIL AND GROUNDWATER MANAGEMENT PLAN (SEE SECTION 1.0 OF THE SGMP). CONTRACTOR SHALL NOTIFY ENGINEER IF THE ABANDONED FUEL SYSTEM STRUCTURES AND PIPING CONTAIN PETROLEUM PRODUCTS PRIOR TO REMOVALS. D. THE BACKFILL FOR PIPE TRENCHES AND STRUCTURES SHALL MEET THE P-152 SPECIFICATION AND SHALL BE INCIDENTAL TO THIS BID ITEM.
7	P-101	
8	P-101	REMOVE AND DISPOSE OF ABANDONED 2-WAY 4 INCH CONCRETE ENCASED DUCT BANK REMOVE AND DISPOSE OF ABANDONED 8-WAY 4 INCH CONCRETE ENCASED DUCT BANK A. REFER TO D-SHEETS FOR ABANDONED CONCRETE ENCASED DUCT BANK LOCATIONS AND DETAILS. B. CAPPING AND SEALING 4 INCH CONDUITS SHALL BE INCIDENTAL TO THESE BID ITEMS. C. REMOVALS SHALL BECOME THE PROPERTY OF THE CONTRACTOR, NO DISPOSAL SITE EXISTS ON THE AIRPORT. D. THE BACKFILL FOR DUCT SHALL MEET THE P-152 SPECIFICATION AND SHALL BE INCIDENTAL TO THIS BID ITEM. EXCAVATIONS AND BACKFILL PER THE RECOMMENDATIONS OF THE SOIL AND GROUNDWATER MANAGEMENT PLAN ARE INCIDENTAL TO THIS ITEM.
9	P-101	
10	P-152	UNCLASSIFIED EXCAVATION A. THIS BID ITEM INCLUDES THE FOLLOWING QUANTITY FOR EXCAVATION OF EXISTING ON-SITE MATERIAL PER THE E-SHEETS: CUT = 6,393 CY (CUT USED FOR ON-SITE FILL SHALL HAVE 1.30 SHRINK FACTOR) B. ALL EXCESS EXCAVATION MATERIAL INCLUDING UNSUITABLE AND NON-CONTAMINATED MATERIAL SHALL BECOME PROPERTY OF THE CONTRACTOR. NO DISPOSAL SITE EXISTS ON AIRPORT PROPERTY OTHER THAN FOR MATERIALS IDENTIFIED AS CONTAMINATED PER BID ITEM 11. C. EXCESS EXCAVATION MATERIAL CLASSIFIED BY THE RPR AND ENVIRONMENTAL TESTING FIRM AS CONTAMINATED SHALL BE HANDLED AND STOCKPILED AS DESCRIBED IN BID ITEM 11. THE QUANTITY OF CONTAMINATED SOIL STOCKPILED ON AIRPORT PROPERTY SHALL BE SUBTRACTED FROM THE ESTIMATED UNCLASSIFIED EXCAVATION VOLUME. D. INCIDENTAL TO THIS BID ITEM, IT IS ANTICIPATED THAT HAULING TO THE STOCKPILE AREA MAY BE NECESSARY TO CONFIRM THE EXCAVATED MATERIAL'S CONTAMINATION STATUS. SHOULD THE MATERIAL BE FOUND NON-CONTAMINATED, ADDITIONAL PAYMENT FOR HAULING OFF-SITE WILL NOT BE RECEIVED.
11	SPECIAL	REMOVE AND DISPOSE OF CONTAMINATED SOIL A. THIS IS AN UNDISTRIBUTED ITEM TO BE USED IF CONTAMINATED SOILS ARE ENCOUNTERED DURING EXCAVATION. CONTRACTOR SHALL NOTIFY THE ENGINEER IF CONTAMINATED SOILS ARE OBSERVED DURING EXCAVATIONS. B. CONTRACTOR SHALL MANAGE, HANDLE, AND STOCKPILE CONTAMINATED SOILS AND GROUNDWATER PER THE RECOMMENDATIONS OF THE SOIL AND GROUNDWATER MANAGEMENT PLAN (SGMP). THIS ITEM IS MEASURED PER CUBIC YARDS (CY) OF CONTAMINATED SOILS THAT REQUIRE THE CONTRACTOR TO STOCKPILE AND CONTAIN THE MATERIAL ON AIRPORT PROPERTY AS SHOWN ON SHEET J.03 AND U.03. ALL LABOR, EQUIPMENT, AND MATERIAL NECESSARY TO IDENTIFY, ISOLATE, HAUL, STOCKPILE, COVER, AND CONTAIN THE CONTAMINATED SOIL SHALL BE INCLUDED IN THE COST OF THIS ITEM. C. THE OWNER SHALL EMPLOY AND PAY FOR AN ENVIRONMENTAL TESTING AGENCY (TERRACON) TO PERFORM ON-SITE PHOTOIONIZATION TESTING (PID) OF SOILS THAT THE CONTRACTOR PERCEIVES TO BE CONTAMINATED. UPON NOTIFICATION, TERRACON SHALL ASSIST IN IDENTIFYING CONTAMINATED SOILS THAT SHALL BE ISOLATED AND HAULED TO THE AIRPORT STOCKPILE LOCATION. CONTRACTOR COORDINATION WITH RPR AND TERRACON IS A REQUIREMENT OF THIS BID ITEM.
14	P-219	RECYCLED CONCRETE AGGREGATE BASE COURSE, 6 INCH A. P-219 JOB MIX FORMULA MAY INCLUDE UP TO 10% ASPHALT MILLINGS. REFER TO SPECIFICATION SECTION P-219-2.1 FOR DETAILS. B. TABULATIONS OF P-219 CRUSHED AGGREGATE AVAILABILITY AND NEEDS LOCATED ON SHEET C.01. TABULATIONS INTENDED TO CONVEY ESTIMATED MATERIAL AVAILABILITY AND PROJECT NEEDS. CONTRACTOR'S OPERATIONS MAY IMPACT ACTUAL AVAILABLE MATERIAL.
16	P-604	COMPRESSION JOINT SEALS JOINT SEALING FILLER A. COMPRESSION JOINT SEALANT SHALL BE APPLIED TO ALL TYPE C AND E JOINTS ON GA APRON PAVEMENT. B. JOINT SEALING FILLER (SILICONE) SHALL BE APPLIED TO ALL A, A-1 AND A-2 JOINTS.
17	P-605	
18	P-620	PAVEMENT MARKINGS - TEMPORARY 6" YELLOW CENTERLINE MARKING PAVEMENT MARKINGS - YELLOW TAXIWAY PAINT, TYPE 1 BEADS PAVEMENT MARKINGS - YELLOW TAXIWAY PAINT, TYPE 3 BEADS PAVEMENT MARKINGS - BLACK OUTLINES, PAINT A. THESE ITEMS SHALL INCLUDE ALL MATERIALS AND EQUIPMENT NECESSARY TO COMPLETE THE PAINTING AS INDICATED ON THE PAVEMENT MARKING PLAN AND IN THE P-620 SPECIFICATION. B. WATERBORNE TYPE I PAINT SHALL BE USED FOR ALL TEMPORARY PAVEMENT MARKINGS. CONTRACTOR MAY SUBSTITUTE TYPE I PAINT WITH TYPE III PAINT FOR TEMPORARY MARKINGS. C. WATERBORNE TYPE III PAINT SHALL BE USED FOR ALL PERMANENT PAVEMENT MARKINGS. D. REFER TO K-SHEETS FOR BEAD APPLICATIONS FOR ALL MARKINGS.
19	P-620	
20	P-620	
21	P-620	
22	D-701	15 INCH RCP, CLASS V, GASKETED A. ALL STORM SEWER PIPE SHALL BE CLASS V REINFORCED CONCRETE PIPE (RCP). B. REFER TO SHEETS M.01 AND M.02 FOR STORM SEWER PLAN AND PROFILES AND SHEET M.03 FOR STORM SEWER TABULATIONS. C. GASKETED PIPE JOINTS SHALL BE INCIDENTAL TO THIS BID ITEM. D. UNCLASSIFIED TRENCH EXCAVATION AND HAULING/DISPOSING OF WASTE MATERIAL SHALL BE INCIDENTAL TO THE UNIT PRICE OF THE PIPE INSTALLED. E. ALL BEDDING, INITIAL BACKFILL AND TRENCH BACKFILL MATERIAL AND INSTALLATION SHALL BE INCIDENTAL TO THE UNIT PRICE OF THE PIPE INSTALLED. REFER TO DETAIL 02 ON SHEET M.05 FOR STORM SEWER TRENCH DETAILS.

ESTIMATE REFERENCE INFORMATION		
ITEM NO.	ITEM CODE	DESCRIPTION
23	D-705	PERFORATED SUBDRAIN, 6 INCH DIA. SUBDRAIN CLEANOUT A. REFER TO SHEETS M.01 AND M.02 FOR SUBDRAIN PLAN AND SHEET M.03 FOR SUBDRAIN AND SUBDRAIN CLEANOUT TABULATIONS. B. UNCLASSIFIED TRENCH EXCAVATION AND HAULING/DISPOSING OF WASTE MATERIAL SHALL BE INCIDENTAL TO THE UNIT PRICE OF THE PIPE INSTALLED. C. ALL FILTER FABRIC, PIPE FITTINGS AND POROUS BACKFILL MATERIAL AND INSTALLATION SHALL BE INCIDENTAL TO THE UNIT PRICE OF THE PIPE INSTALLED. D. ALL PIPE, FITTINGS, CASTINGS AND REINFORCEMENT FOR SUBDRAIN CLEANOUTS SHALL BE INCIDENTAL TO THE SUBDRAIN CLEANOUT BID ITEM. E. REFER TO SHEETS M.03 AND M.04 FOR SUBDRAIN AND SUBDRAIN CLEANOUT DETAILS.
24	D-705	
25	D-751	MODIFIED SW-511 SINGLE GRATE INTAKE A. UNCLASSIFIED EXCAVATION AND HAULING/DISPOSING OF WASTE MATERIAL SHALL BE INCIDENTAL TO THE UNIT PRICE OF THIS BID ITEM. B. ALL BEDDING AND BACKFILL MATERIAL SHALL BE INCIDENTAL TO THE UNIT PRICE OF THIS BID ITEM. C. MODIFIED SW-511 SINGLE GRATE INTAKES SHALL BE INSTALLED PER DETAIL 01 ON SHEET M.06.
26	D-751	SW-211, STORM SEWER CONNECTION TO EXISTING PIPE A. ALL MATERIAL, EQUIPMENT AND LABOR TO INSTALL A TYPE PC-2 CONCRETE COLLAR AROUND THE PROPOSED 15 INCH RCP STORM SEWER TO THE EXISTING 12 INCH RCP. STORM SEWER SHALL BE INCIDENTAL TO THIS BID ITEM. B. REFER TO SHEET M.02 FOR SW-211 STORM SEWER CONNECTION LOCATION AND SHEET M.05 FOR SW-211 STORM SEWER CONNECTION DETAIL.
27	SPECIAL	PERMANENT STORM INTAKE POLLUTION PREVENTION DEVICE IN MODIFIED SW-511 SINGLE GRATE INTAKE A. PERMANENT STORM INTAKE POLLUTION PREVENTION DEVICE SHALL BE SAFE DRAIN CONTAINMENT SYSTEM WITH 8" VALVE AND 8" HYDROCARBON FILTRATION FILTER FOR 35.5" X 23.5" INLET FRAME OR APPROVED EQUIVALENT. B. CONTRACTOR SHALL ENSURE PERMANENT STORM INTAKE POLLUTION PREVENTION DEVICES ARE PROPERLY SIZED FOR THE INTAKE GRATES SELECTED FOR BID ITEM 25 (MODIFIED SW-511 SINGLE GRATE INTAKE). PERMANENT STORM INTAKE POLLUTION PREVENTION DEVICES SHALL BE INSTALLED PER THE MANUFACTURER'S RECOMMENDATIONS. C. ALL MATERIAL, EQUIPMENT AND LABOR TO PROVIDE AND INSTALL PERMANENT STORM INTAKE POLLUTION PREVENTION DEVICES SHALL BE INCIDENTAL TO THIS BID ITEM.
28	C-100	CONTRACTOR QUALITY CONTROL PROGRAM A. THIS ITEM INCLUDES BUT IS NOT LIMITED TO ALL COSTS ASSOCIATED WITH DEVELOPING AND IMPLEMENTING A QUALITY CONTROL PROGRAM THAT DETAILS METHODS AND PROCEDURES THAT WILL BE TAKEN TO ASSURE THAT ALL MATERIAL AND COMPLETED CONSTRUCTION, INCLUDING THAT WHICH IS PERFORMED BY SUBCONTRACTORS REQUIRED BY THIS CONTRACT CONFORM TO THE CONTRACT PLANS, TECHNICAL SPECIFICATIONS, AND APPLICABLE STATE AND FEDERAL REQUIREMENTS. B. REFER TO SPECIFICATION C-100 FOR PROGRAM REQUIREMENTS AND EXECUTION.
29	C-102	STORM WATER POLLUTION PREVENTION PLAN (SWPPP), PREPARATION AND MANAGEMENT A. THE UNIT PRICE SHALL BE FULL COMPENSATION FOR FURNISHING ALL MATERIALS, LABOR, AND EQUIPMENT NECESSARY TO MAINTAIN THE EROSION CONTROL PLAN DURING CONSTRUCTION AT BOTH THE RECONSTRUCTION SITE AND THE STAGING/BATCH PLANT AREA. B. THIS ITEM INCLUDES PLAN PREPARATION, NPDES GENERAL PERMIT #2, MANAGEMENT, INSPECTIONS, INSTALLATION AND MAINTENANCE OF SILT FENCE AND OTHER CONTROL MEASURES, AND ALL ADDITIONAL POLLUTION CONTROL MEASURES THAT WILL BE CONSTRUCTED AND MAINTAINED BY THE CONTRACTOR BASED ON THE ON THE CONTRACTOR'S STORM WATER POLLUTION PREVENTION PLAN. C. THE CONTRACTOR SHALL SUBMIT A DETAILED STORM WATER POLLUTION PLAN FOR BOTH THE RECONSTRUCTION SITE AND THE STAGING/BATCH PLANT AREA FOR REVIEW BY THE ENGINEER, PRIOR TO COMMENCEMENT OF CONSTRUCTION ACTIVITIES. D. PARTIAL PAYMENT FOR THIS ITEM SHALL BE BASED ON THE ESTIMATED PERCENTAGE OF WORK COMPLETED AT THE TIME OF SUBMITTAL OF THE PAY APPLICATION. E. FINAL PAYMENT WILL BE MADE AFTER ALL WORK ON THE PROJECT REQUIRED BY THE CONTRACT HAS BEEN COMPLETED, 90% OR GREATER VEGETATED COVERAGE HAS BEEN ACHIEVED, AND NOTICE OF DISCONTINUATION HAS BEEN FILED.
30	C-102	TEMPORARY INLET PROTECTION DEVICE A. THIS BID ITEM IS FOR THE INSTALLATION AND MAINTANCE OF ALL TEMPORARY INLET PROTECTION DEVICES WITHIN THE GA APRON RECONSTRUCTION LIMITS. REFER TO SHEETS D.01, M.01 AND M.02 FOR EXISTING AND PROPOSED INTAKE LOCATIONS. B. CONTRACTOR SHALL ENSURE ELECTED TEMPORARY INLET PROTECTION DEVICE MEASURES PREVENT SILTS FROM LEAVING THE CONSTRUCTION SITE AND ARE IN COMPLIANCE WITH SPECIFICATION C-102 AND THE CONTRACTOR'S STORM WATER POLLUTION PREVENTION PLAN (SWPPP).
31	C-105	MOBILIZATION AND MISCELLANEOUS A. REFER TO SPECIFICATION C-105 FOR MOBILIZATION AND MISCELLANEOUS BID ITEM REQUIREMENTS. B. MOBILIZATION IS LIMITED TO 10% OF THE TOTAL COST OF THE PROJECT, REFER TO SPECIFICATION C-105 FOR PAYMENT SCHEDULE.
32	C-105	TRAFFIC CONTROL AND SECURITY A. REFER TO SPECIFICATION C-105 FOR TRAFFIC CONTROL AND SECURITY BID ITEM REQUIREMENTS. B. THIS BID ITEM INCLUDES COMPENSATION FOR ALL SECURITY ITEMS REQUIRED FOR CONSTRUCTION. THIS INCLUDES BUT IS NOT LIMITED TO EMPLOYEE BADGING, BACKGROUND CHECKS, GATE GUARDS, ETC. THE AIRPORT BADGING FEE IS \$60 PER INDIVIDUAL. C. ALL TRAFFIC CONTROL AS SHOWN ON THE J-SHEETS SHALL BE INCLUDED WITH THIS ITEM. THESE ITEMS INCLUDE, BUT ARE NOT LIMITED TO, LOW-PROFILE BARRICADES, 48" CHANNELIZERS, TEMPORARY SIGNAGE, AND MAINTENANCE OF ALL TRAFFIC CONTROL, AMONG OTHERS. D. REFER TO SPECIFICATION C-105 FOR PAYMENT SCHEDULE.
33	C-105	CONSTRUCTION ACCESS A. REFER TO SPECIFICATION C-105 FOR CONSTRUCTION ACCESS BID ITEM REQUIREMENTS. B. ITEM INCLUDES ALL MODIFICATIONS TO PROJECT STAGING/BATCH PLANT AREA AS DEEMED NECESSARY BY THE CONTRACTOR PRIOR TO CONSTRUCTION. THIS ITEM ALSO INCLUDES ANY FINAL RESTORATION OR CLEAN-UP OF THE STAGING AREA AS DEEMED NECESSARY BY THE CONTRACTOR/OWNER UPON COMPLETION OF CONSTRUCTION.
34	GP-50	CONSTRUCTION SURVEY A. REFER TO GENERAL PROVISIONS 50-07 FOR THE REQUIREMENTS OF THIS BID ITEM. CONTRACTOR SHALL NOTE GRADE TOLERANCES WITHIN THE INDIVIDUAL SPECIFICATION SECTIONS, WITH PARTICULAR ATTENTION TO THOSE INCLUDED IN THE PAVEMENT SECTION. GRADE CHECKS WILL BE PERFORMED BY THE ENGINEER TO VERIFY CONTRACTOR'S CONSTRUCTION IS WITHIN SAID TOLERANCES LISTED IN THE INDIVIDUAL TECHNICAL SPECIFICATIONS.

1

FAA/DOT AIP: 3-19-0028-081/91240DBQ100 FOTH PROJECT NO: 0022D023.04
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RECONSTRUCT GA APRON
PHASE 1 CONSTRUCTION
DUBUQUE, IOWA

ESTIMATE REFERENCE INFORMATION

SHEET NO.

C.02

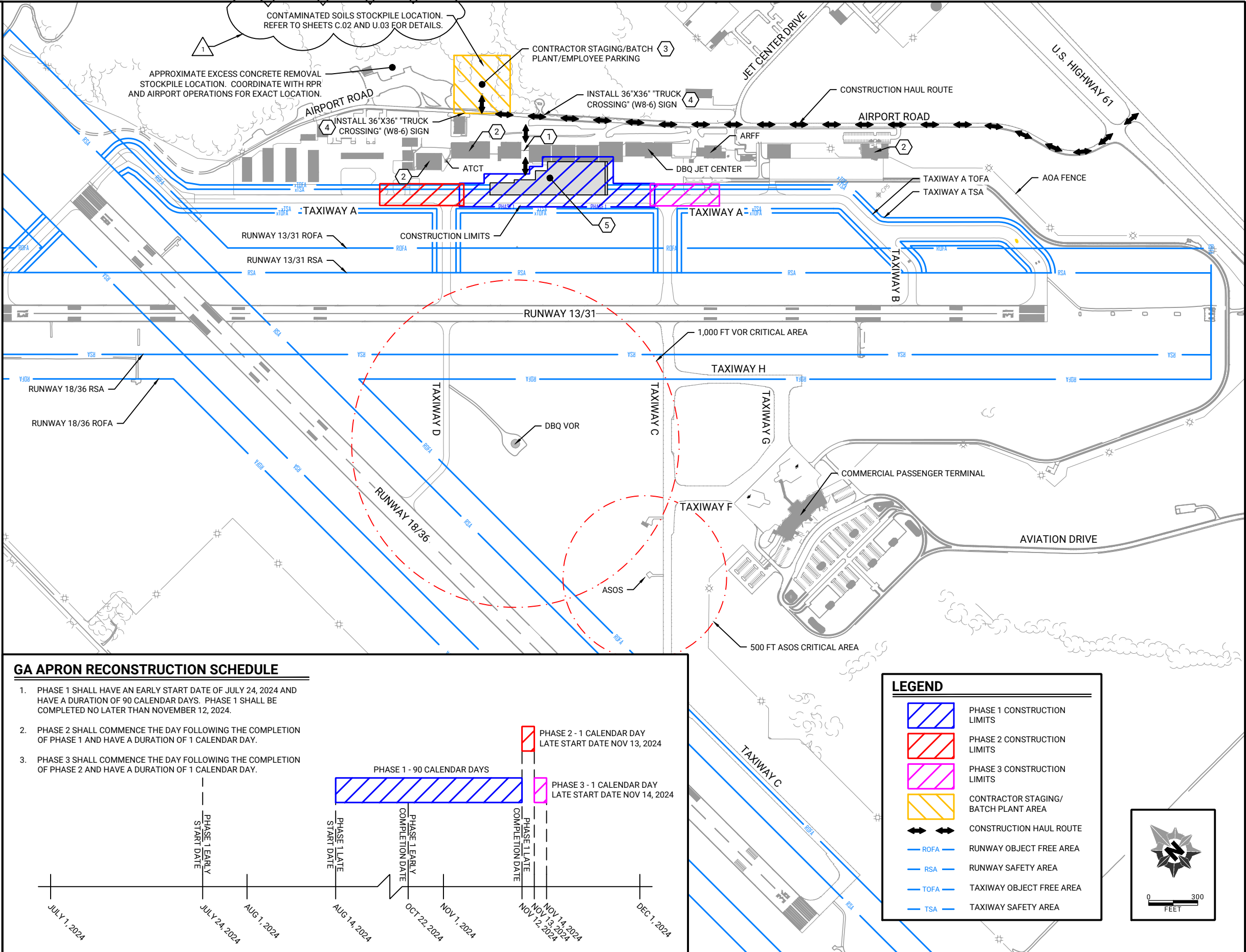
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STAGING/BATCH PLANT AREA NOTES

1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL MODIFICATIONS TO THE STAGING SITE. UPON COMPLETION OF THE PROJECT, THE CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVING ALL RUBBISH, FENCING, AND CONSTRUCTION DEBRIS, GRADING THE AREA TO DRAIN, AND TURFING. NO ADDITIONAL PAYMENT WILL BE MADE FOR ANY WORK AT THE STAGING/BATCH PLANT SITE. ALL WORK ASSOCIATED WITH IMPROVEMENTS TO THE STAGING SITE SHALL BE CONSIDERED INCIDENTAL TO THE CONSTRUCTION ACCESS BID ITEM. THE CONTRACTOR SHALL SUBMIT A PLAN FOR THE STAGING/BATCH PLANT LAYOUT TO THE ENGINEER FOR APPROVAL 30 DAYS PRIOR TO THE START OF WORK. THE CONTRACTOR SHALL OBTAIN ALL NECESSARY LOCAL, STATE, AND FEDERAL PERMITS.
2. POTENTIAL CONTRACTOR BATCH PLANT WILL BE SUBMITTED TO THE FAA FOR AIRSPACE ANALYSIS REVIEW. IF A CONTRACTOR MODIFIES THE LOCATION OR HEIGHTS OF THE BATCH PLANT THE MUST RESUBMIT A NEW 7460-1 TO THE FAA FOR AIRSPACE ANALYSIS REVIEW. CONTRACTOR SHALL SUBMIT A MINIMUM OF 60 DAYS PRIOR TO USE. INFORMATION REGARDING THE SUBMITTAL OF AIRSPACE ANALYSIS CAN BE FOUND AT THE FOLLOWING: WWW.OEAAA.FAA.GOV/OEAAA/EXTERNAL/PORTAL.JSP
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL UTILITIES IN THE VICINITY OF THE STAGING/BATCH PLANT AREA PRIOR TO OCCUPYING THE SITE. PRIOR TO THE START OF CONSTRUCTION, THE CONTRACTOR SHALL COORDINATE WITH ALL OF THE APPROPRIATE UTILITY COMPANIES FOR USE OF UTILITIES NEEDED FOR CONSTRUCTION. SPECIAL CARE SHALL BE MAINTAINED TO NOT DAMAGE EXISTING AIRPORT STORM SEWER OUTFALL INFRASTRUCTURE NEAR THE STAGING/BATCH PLANT AREA.
4. THE CONTRACTOR SHALL PROVIDE ACCEPTABLE DUST PREVENTION MEASURES TO MINIMIZE THE IMPACT ON NEIGHBORING AREAS. DUST PREVENTION MEASURES SHALL INCLUDE BUT ARE NOT LIMITED TO, CONTINUOUS WETTING OF ALL EXPOSED AREAS AND CONTINUOUS CLEANING OF ALL PAVEMENT/HAUL ROUTES USING AN APPROVED VACUUM TYPE SWEEPER.
5. THE CONTRACTOR SHALL CONTROL RUNOFF OF ANY IMPROVEMENTS MADE TO THE STAGING/BATCH PLANT SITE IN ACCORDANCE WITH LOCAL, STATE, AND FEDERAL REGULATIONS.
6. THE CONTRACTOR SHALL SUBMIT FOR REVIEW TO THE ENGINEER A STORM WATER POLLUTION PREVENTION PLAN (SWPPP) FOR THE STAGING/BATCH PLANT SITE. THE SWPPP SHALL CONTAIN AT LEAST THE MINIMUM SWPPP REQUIREMENTS OF THE IOWA STATE DEPARTMENT OF NATURAL RESOURCES.
7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ACCESS TO PUBLIC ROADWAYS WITH THE RESPONSIBLE AGENCIES.
8. CONTRACTOR VEHICLES AND EQUIPMENT OPERATING ON AIRPORT ROADWAYS SHALL NOT EXCEED MAXIMUM LEGAL WEIGHTS. REPLACEMENT OF DAMAGED AIRPORT PAVEMENTS DUE TO VEHICLE OVERLOADING SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO REPLACE AT NO ADDITIONAL COST TO THE OWNER.

REFERENCED NOTES

1. CONTRACTOR SHALL ACCESS THE CONSTRUCTION SITE THROUGH THE EXISTING 16-FOOT MANUAL VEHICLE GATE LOCATED BETWEEN HANGARS 93 AND 96. CONTRACTOR RESPONSIBLE FOR FOR HAVING GATE GUARD PRESENT AT THE CONSTRUCTION ACCESS GATE FOR ACCESS CONTROL OF CONSTRUCTION MATERIALS, EQUIPMENT, AND PERSONNEL WHEN THE GATE IS OPENED OR UNLOCKED.
2. THE UNIVERSITY OF DUBUQUE OPERATES OUT OF THESE BUILDINGS. TRAFFIC MY PEAK AT TIMES ALONG AIRPORT ROAD. CONTRACTOR SHALL ENSURE THAT ALL CONSTRUCTION TRAFFIC ENTERING AND LEAVING THE STAGING AND CONSTRUCTION SITE FOLLOW POSTED SPEED LIMITS AND TAKE EXTRA CAUTION WHEN ENTERING/EXITING AIRPORT ROAD.
3. CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING STAGING/BATCH PLANT AREA (RELOCATING EXISTING EQUIPMENT/MATERIALS, TOPSOIL STRIP/SALVAGE/RESPREAD, EARTHWORK, GRAVEL PAD ADDITIONS OR OTHER SITE WORK THAT THE CONTRACTOR OR RPR DEEMS NECESSARY). UPON COMPLETION OF THE PROJECT, THE CONTRACTOR SHALL RESTORE THE STAGING/BATCH PLANT AREA TO IT'S ORIGINAL CONDITION.
4. CONTRACTOR SHALL INSTALL 36"X36" "TRUCK CROSSING" IN ACCORDANCE WITH THE MOST RECENT EDITION OF THE MUTCD. SIGNS SHALL BE PLACED A MINIMUM OF 100 FEET PRIOR TO THE CONSTRUCTION SITE AND STAGING/BATCH PLANT ENTRANCES.
5. CONTRACTOR SHALL ONLY FUEL TRACKED EQUIPMENT WITHIN THE AOA. ALL OTHER EQUIPMENT/VEHICLES SHALL BE FUELED AT THE STAGING/BATCH PLANT SITE.



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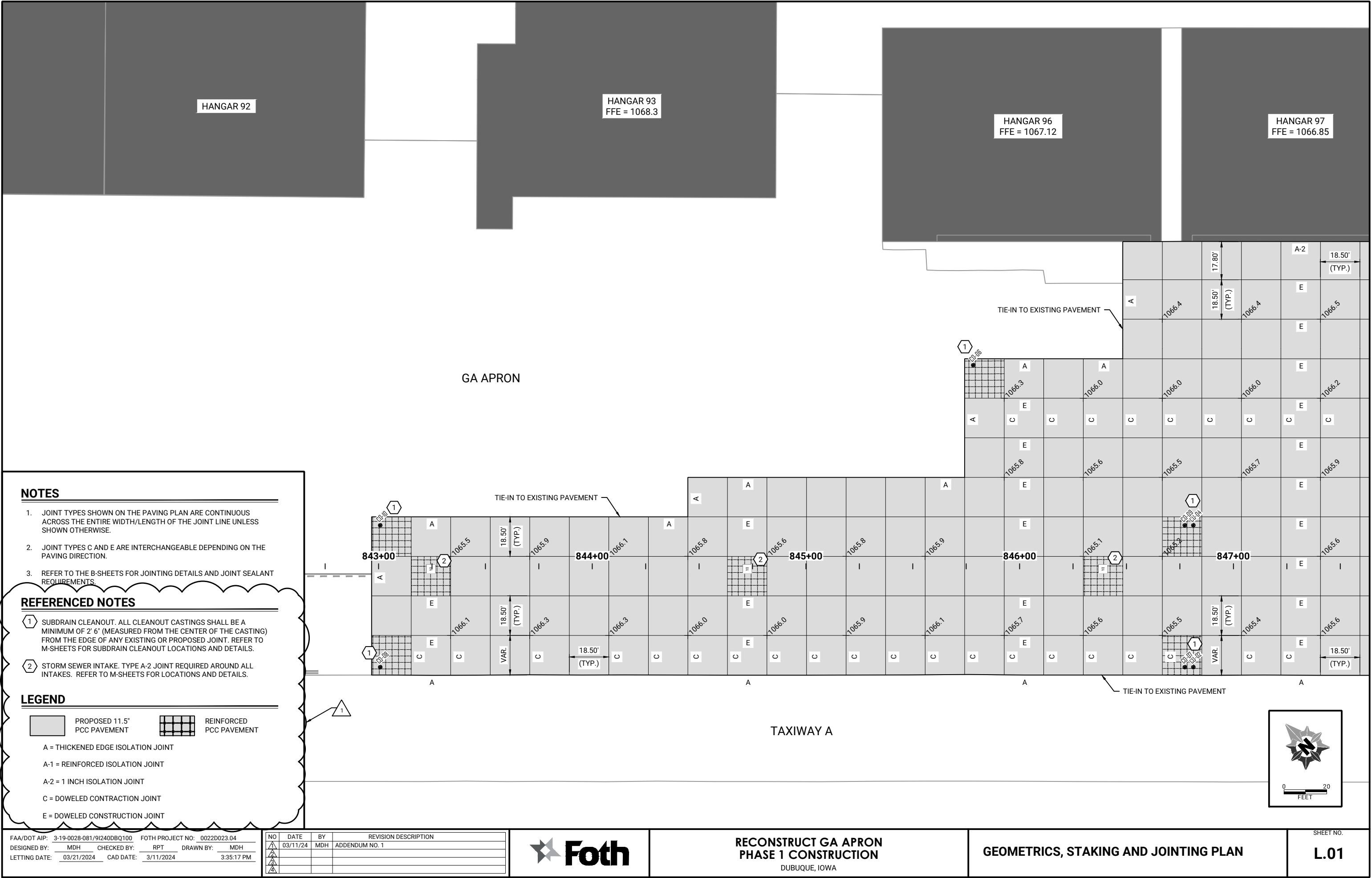
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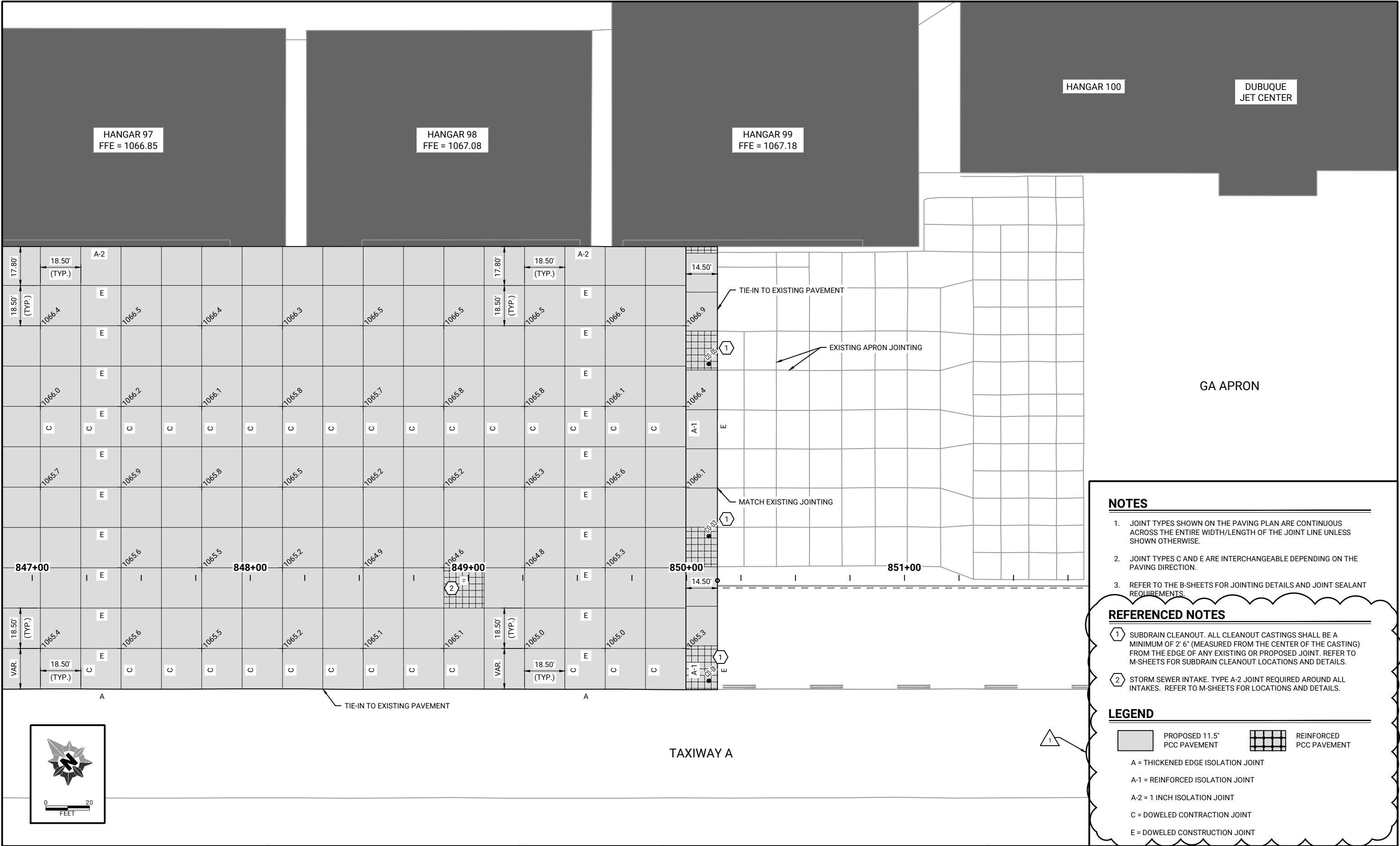


RECONSTRUCT GA APRON
PHASE 1 CONSTRUCTION
DUBUQUE, IOWA

CONSTRUCTION SAFETY AND PHASING PLAN
OVERALL SAFETY AND PHASING PLAN

SHEET NO.
J.03





NOTES

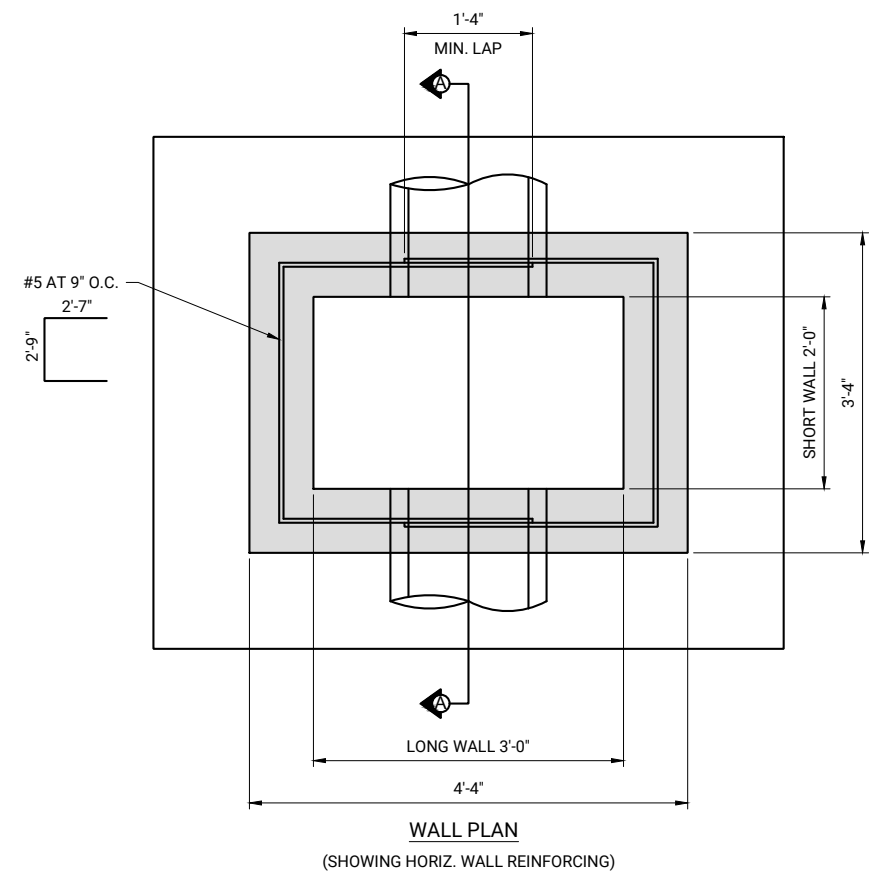
- JOINT TYPES SHOWN ON THE PAVING PLAN ARE CONTINUOUS ACROSS THE ENTIRE WIDTH/LENGTH OF THE JOINT LINE UNLESS SHOWN OTHERWISE.
- JOINT TYPES C AND E ARE INTERCHANGEABLE DEPENDING ON THE PAVING DIRECTION.
- REFER TO THE B-SHEETS FOR JOINTING DETAILS AND JOINT SEALANT REQUIREMENTS.

REFERENCED NOTES

- SUBDRAIN CLEANOUT. ALL CLEANOUT CASTINGS SHALL BE A MINIMUM OF 2' 6" (MEASURED FROM THE CENTER OF THE CASTING) FROM THE EDGE OF ANY EXISTING OR PROPOSED JOINT. REFER TO M-SHEETS FOR SUBDRAIN CLEANOUT LOCATIONS AND DETAILS.
- STORM SEWER INTAKE. TYPE A-2 JOINT REQUIRED AROUND ALL INTAKES. REFER TO M-SHEETS FOR LOCATIONS AND DETAILS.

LEGEND

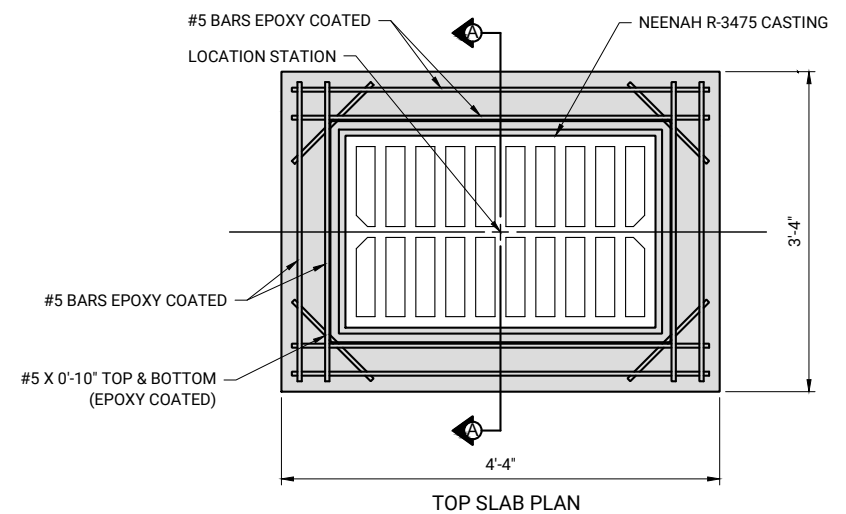
- PROPOSED 11.5" PCC PAVEMENT
- REINFORCED PCC PAVEMENT
- A = THICKENED EDGE ISOLATION JOINT
- A-1 = REINFORCED ISOLATION JOINT
- A-2 = 1 INCH ISOLATION JOINT
- C = DOWELED CONTRACTION JOINT
- E = DOWELED CONSTRUCTION JOINT



1. FOUNDATION MATERIAL SHALL BE COMPACTED TO 95% MODIFIED DENSITY (ASTM D 1557).

- 1 INSTALL FOUR #4 DIAGONAL BARS AT ALL PIPE OPENINGS.
- 2 12-INCH MINIMUM WALL HEIGHT ABOVE ALL PIPES.
- 3 TYPE "A-2" 1 INCH ISOLATION JOINT. REFER TO B-SHEETS AND L-SHEETS FOR DETAILS.

MAXIMUM PIPE DIAMETERS		
PIPE LOCATION	PRECAST STRUCTURE	CAST-IN-PLACE STRUCTURE
SHORT WALL	15"	18"
LONG WALL	24"	30"



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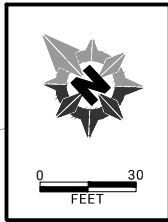
RECONSTRUCT GA APRON PHASE 1 CONSTRUCTION

STORM SEWER AND SUBDRAIN STORM INTAKE DETAILS

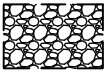
MODIFIED		REVISION	
		2	04-21-20
FIGURE 6010.511	STANDARD ROAD PLAN	SW-511	
MODIFICATIONS: Increased wall thickness and base size and added a thicker top slab with a different casting.			
RECTANGULAR AREA INTAKE			

SHEET NO. _____

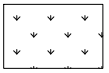
M.06



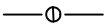
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STABILIZED CONSTRUCTION
ENTRANCE



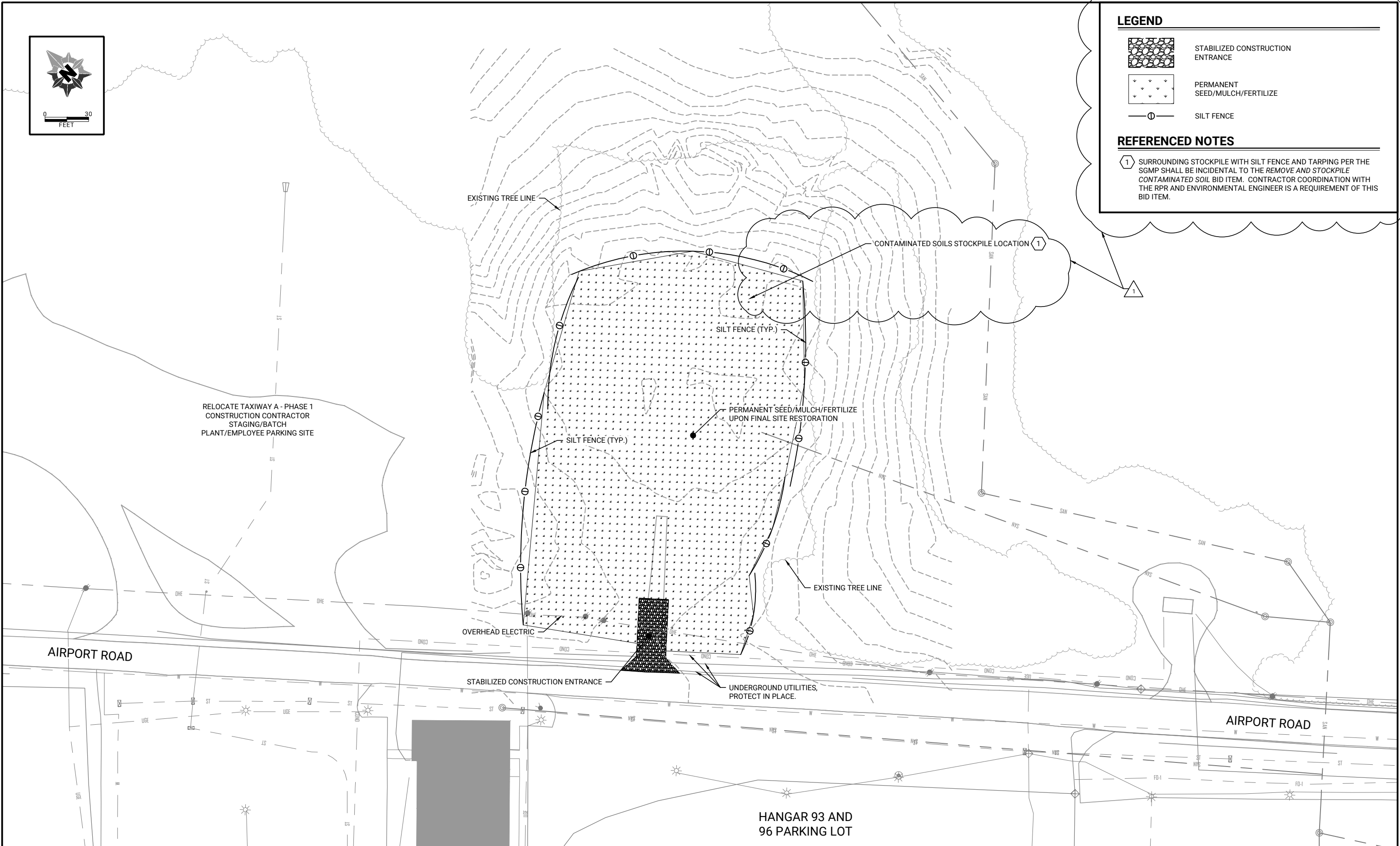
PERMANENT
SEED/MULCH/FERTILIZE



SILT FENCE

REFERENCED NOTES

- 1 SURROUNDING STOCKPILE WITH SILT FENCE AND TARPING PER THE SGMP SHALL BE INCIDENTAL TO THE REMOVE AND STOCKPILE CONTAMINATED SOIL BID ITEM. CONTRACTOR COORDINATION WITH THE RPR AND ENVIRONMENTAL ENGINEER IS A REQUIREMENT OF THIS BID ITEM.



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		RECONSTRUCT GA APRON PHASE 1 CONSTRUCTION DUBUQUE, IOWA		STORM WATER POLLUTION PREVENTION PLAN (SWPPP) - CONTRACTOR STAGING/BATCH PLANT		SHEET NO. U.03
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